



NEW APPLICATION



0000162312

JOHN S. HAMKOWSKI, Director
Dallas Hammit, State Engineer
Steve Boschen, Division Director

Tuesday, June 16, 2015

RECEIVED
AZ CORP COMMISSION
DOCKET CONTROL

2015 JUN 17 PM 3 02

Arizona Corporation Commission
Office of Railroad Safety
Attn: Chris Watson
1200 W Washington Street
Phoenix, AZ 85007

RR-02635B-15-0197

RE: APPLICATION TO INSTALL A NEW GRADE SEPARATED CROSSING AND REMOVE AN EXISTING PUBLIC AT-GRADE CROSSING

Project: CONSTRUCT BELL ROAD TRAFFIC INTERCHANGE

Location: US 60 (MP 143) and Bell Road (City of Surprise & Maricopa County, Arizona)

Federal Project # RARF-060-B-NFA

ADOT Tracs # 060 MA 142 H848501C

Remove crossing AAR/DOT # 025392A

RRMP: 0172.01

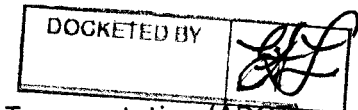
Install new grade-separated crossing

Arizona Corporation Commission
DOCKETED

JUN 17 2015

ORIGINAL

Mr. Watson,



This application is being submitted to allow the Arizona Department of Transportation (ADOT) to construct and maintain a new grade separated structure and to allow the BNSF Railway (BNSF) to remove an existing at-grade public crossing in the City of Surprise, Arizona.

1. Project Location and Description

The new Bell Road traffic Interchange will be located in approximately the same location as the existing Bell Road at-grade crossing, AAR/DOT #025392A. The new grade separations part of the US 60, Bell Road Traffic Interchange project for the Arizona Department of Transportation (ADOT). The entire project is located in Surprise, Arizona.

Plans call for ADOT to construct a new Bell Road bridge to travel over both Grand Avenue and the BNSF tracks. Traffic on Grand Avenue will no longer have to stop, and Grand will connect to the new Bell Road Bridge via on- and off-ramps traveling to and from Grand's center median area.

Additional improvements that will be completed as part of this project within BNSF Railway right-of-way include pier construction for the overpass bridge construction and utility relocations.

The existing at-grade crossing is under the jurisdiction of the City of Surprise, but will be transferred to ADOT in the near future to allow ADOT to construct the project. A Town

resolution in support of removing the at-grade crossing will be obtained and included as part of the executed ADOT/BNSF Construction & Maintenance Agreement.

The project will utilize a design-build delivery method and is currently in the procurement process. Construction is anticipated to begin in January of 2016. The anticipated construction duration is 18 months. Bell Road is within the limits of the BNSF Railway at-grade crossing. To facilitate construction activities to either side of the at-grade crossing, ADOT is requesting that the BNSF be allowed a minimum of 36 months to remove the at-grade crossing after issuing the order.

The project website is: [http://www.azdot.gov/projects/phoenix-metro-area/us-60-\(grand-avenue\)-and-bell-road-interchange](http://www.azdot.gov/projects/phoenix-metro-area/us-60-(grand-avenue)-and-bell-road-interchange)

2. Why the crossing is needed

The Final Design Concept Report entitled US 60; Grand Avenue, Bell Road Traffic Interchange was completed by the Arizona Department of Transportation in January 2015. The report identifies a new grade-separated crossing for Bell Road at US 60 (Grand Avenue) for the need to "eliminate vehicle conflicts with the BNSF Railway east-west main line tracks."

The Design Concept Report can be accessed via the internet at:

[http://azdot.gov/projects/phoenix-metro-area/us-60-\(grand-avenue\)-and-bell-road-interchange/documents](http://azdot.gov/projects/phoenix-metro-area/us-60-(grand-avenue)-and-bell-road-interchange/documents) [UR1]

3. Construction Phasing

Construction is expected to begin in January of 2016. The new grade separation is planned to be constructed in phases. The first phase will temporarily widen Bell Road on the north side to accommodate two lanes in each direction within the north half of Bell Road. Before traffic can be shifted to the north, the existing at-grade railroad crossing will require modifications. Bell Road will be closed to traffic for 28 days to allow BNSF to complete modifications. When the modified crossing is open to traffic, work will begin on the south half of the grade separation. When complete, traffic will be shifted onto the newly constructed roadway while the north half of the grade separation is constructed. Traffic may continue to use the at-grade crossing until the project is complete. Once the grade separation is complete, and in service, the at-grade crossing may be removed. The ADOT contractor will coordinate and receive approval from the BNSF Railway to use the crossing during construction.

4. Maintenance of the grade separation

ADOT will be responsible for constructing and maintaining the Bell Road grade separation over the BNSF Railway right-of-way. BNSF Railway will be responsible for

modifying the at-grade crossing during construction, removing the at-grade crossing and maintaining their infrastructure.

5. Project Funding

The project is funded by the Arizona Department of Transportation (ADOT).

This project is estimated to cost approximately \$66.6M. The portion of the bridge within BNSF Railway right-of-way estimated to cost \$3.4M. The BNSF Railway contribution to the project has not been finalized. The costs for modifications to the existing at-grade railroad crossing will be the responsibility of ADOT. The costs for removal of the at-grade crossing will be the responsibility of BNSF Railway.

6. Other information (based on typical Staff Data Requests):

CW1-1 2012 ADT for Bell Road – crossing US 60: 44,100 vpd.

CW1-2 2014 Intersection LOS at Bell Road/Grand Avenue: LOS D

CW1-3 Past Traffic Studies: A Traffic Report for the project was finalized in January 2015.

CW1-4 Population: 2013 estimated population of City of Surprise is 123,546 persons (source: United states Census Bureau)

CW1-5 Existing Railroad Safety Devices: Bell Road at-grade crossing is currently protected by cantilevered flashing lights and gates for both directions of travel on Bell Road and the right turn lane from Westbound Grand Avenue.

CW1-6 Adjacent Public Crossings: The nearest public crossings of the BNSF Railway are as follows: (1) Dysart Road (DOT # **025393G**); 0.5 miles to the southeast of Bell Road and is at-grade; (2) Greenway Road (DOT # **025651J**), 1.50 miles to the southeast of Bell Road and is at-grade; (3) Meeker BLVD (DOT # **025583k**), 1.87 miles to the northeast of Bell Road and is at-grade.

CW1-7 Grade Separation Study: The Final Design Concept Report, entitled US 60, Grand Avenue Bell Road Traffic Interchange (dated January 2015), recommends a grade-separation of Bell Road and BNSF Railway.

CW1-8 Grade Separation Cost: Cost of Entire project provided above.

CW1-9 Surrounding Zoning: Surrounding area is zoned for Light Industrial, Regional Commercial, and Planned Area Development (source: City of Surprise)

CW1-10 Existing Train Movements: Per FRA website based on 2014 information, 11 freight trains per day. All train movements are thru movements.

CW1-11 Nearest Schools: (1) West Point Elementary School: southwest of crossing (16551 North Dysart Road, Surprise, AZ 0.6 mi SE); (2) Valley Vista High School (15550 North Parkview Place, Surprise, AZ 2.2 mi SW) (3) Surprise

Elementary School 1.4 Miles SE (12907 West Greenway Road El Mirage,
Arizona 85335)

- CW1-12 Nearest Hospitals: (1) Banner Del E Webb Medical Center - 14502 West Meeker Boulevard, Sun City West, AZ 85375 1.9 mi (2) Northwest VA Health Care Clinic - 13985 West Grand Avenue #101, Surprise, AZ 85374
- CW1-13 Railroad Costs: To be determined
- CW1-14 Hazardous Materials traffic: yes but It is not known how much hazardous materials traffic uses the existing crossing each day.
- CW1-15 Speed Limit: Bell Road has a posted speed limit of 45 mph.
- CW1-16 School bus use: estimated 19 per day
- CW1-17 Other bus use: Unknown.
- CW1-18 Abandoned Railroad Spurs: Unknown if railroad spurs have been removed within a 10 mile radius.
- CW1-19 Summary of FHWA grade separation guidelines: Does not apply. This crossing will be grade separated.
- CW1-20 Expected traffic delay due to train traffic: Does not apply. This crossing will be grade separated.

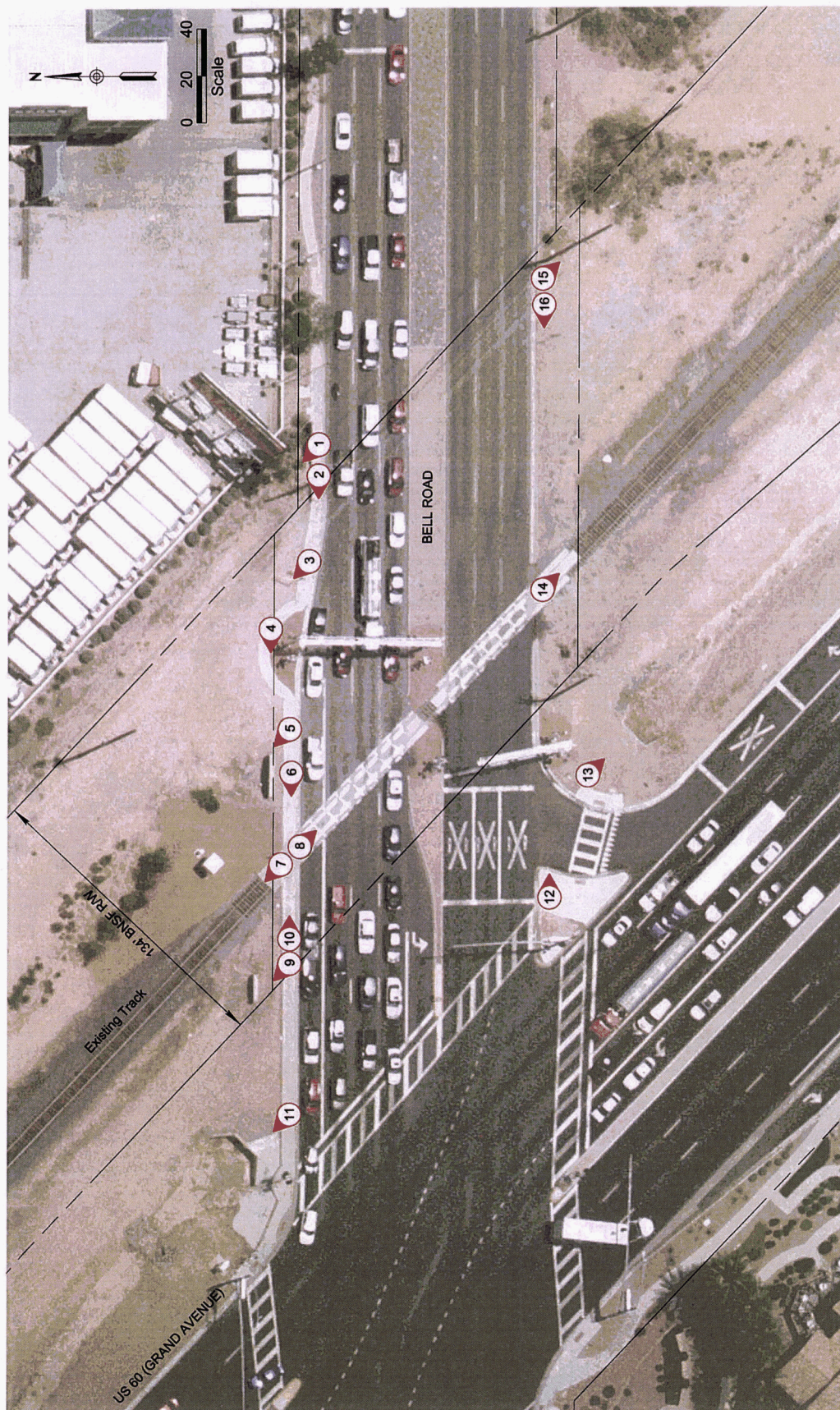
Sincerely,

Sayed M. Hani

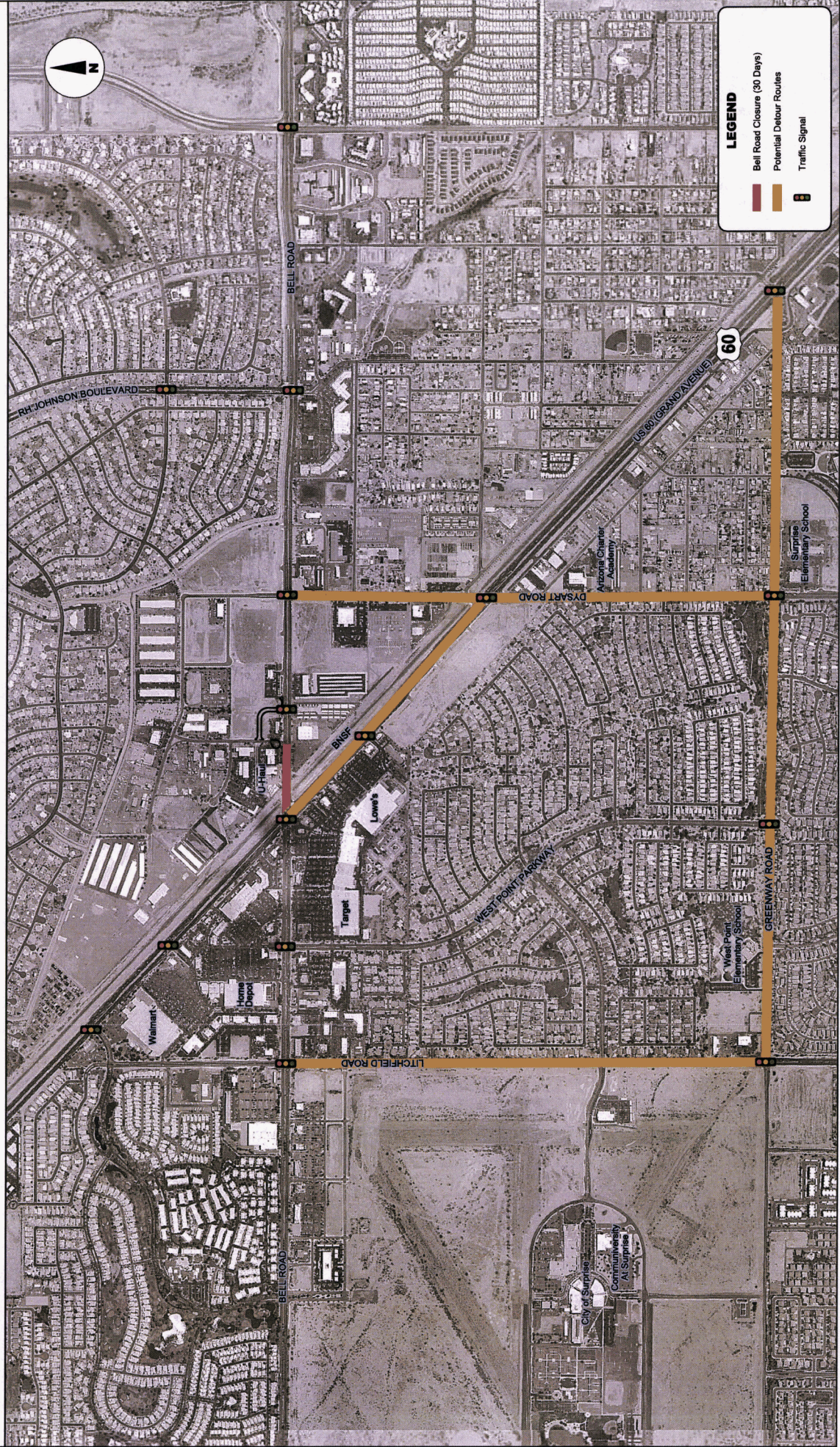
Sayed M. Hani
Utility Engineering Coordinator/ **Railroad Liaison**
205 S 17TH AVE, MD 618E
PHOENIX, AZ 85007
602.712.7555
Shani@azdot.gov

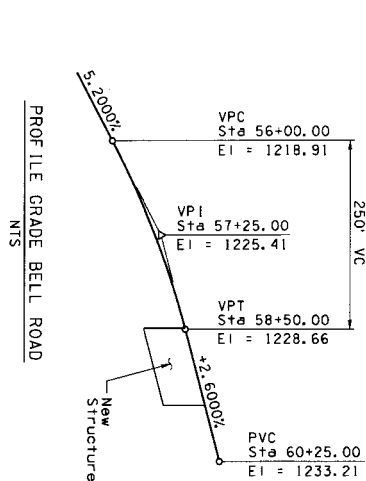
ADOT
Intermodal Transportation

Photograph Locations

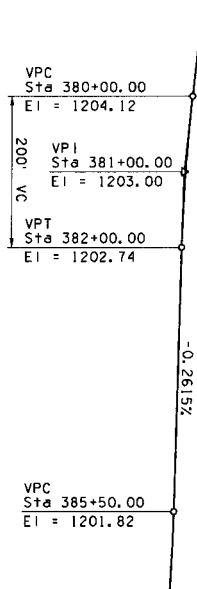


BELL ROAD CLOSURE US 60 (Grand Avenue) / Bell Road Traffic Interchange

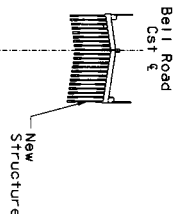




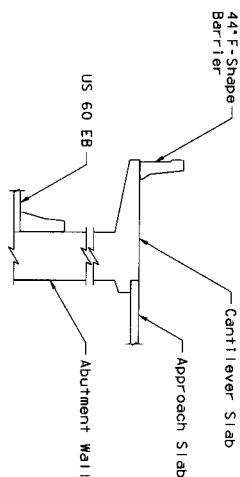
PROFILE GRADE BELL ROAD



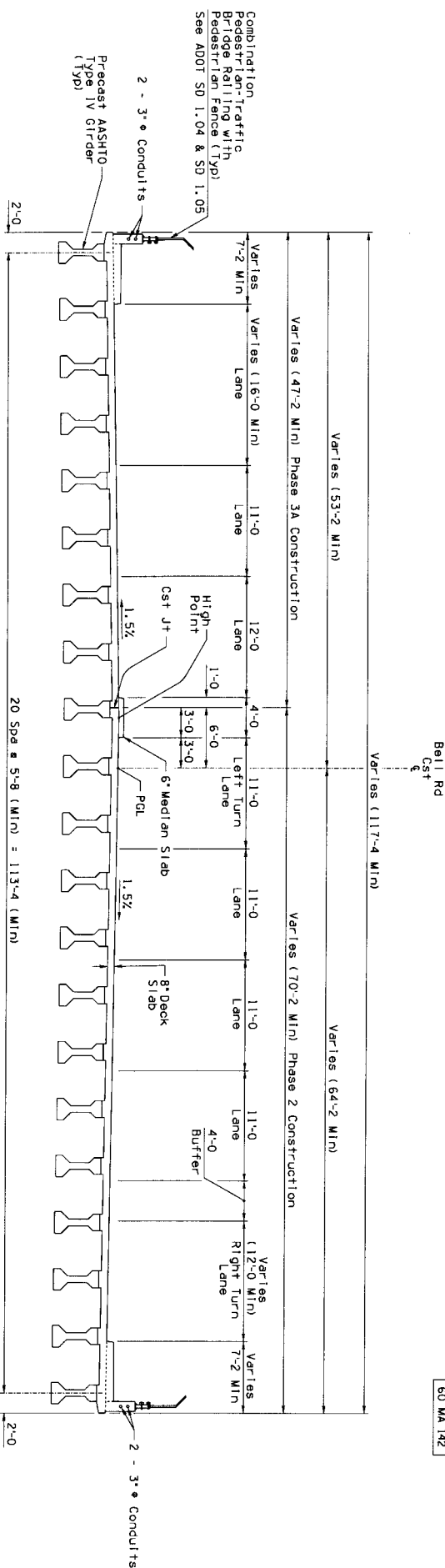
PROFILE GRADE EB US 60



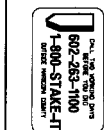
TYPICAL SECTION
Scale: 1" = 5'-0"



CANTILEVER SLAB SECTION
Scale: 1" = 5'-0"



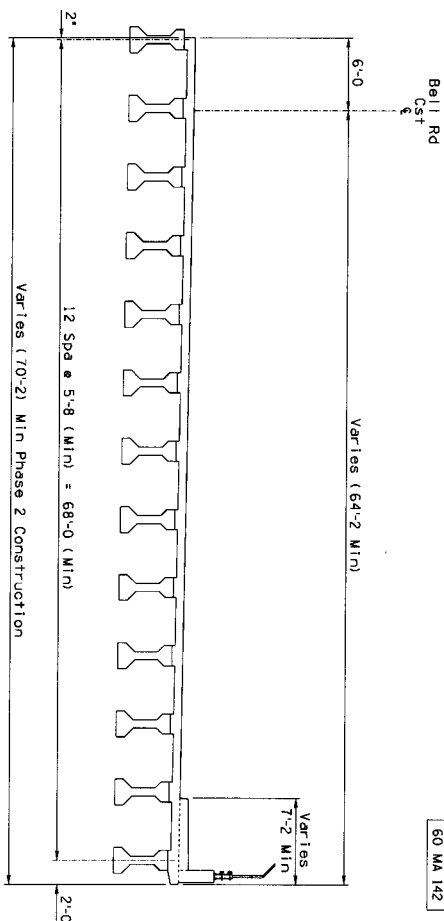
FEDERAL REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	REC. DMS.
9	ARIZ.	060-B-NFA			



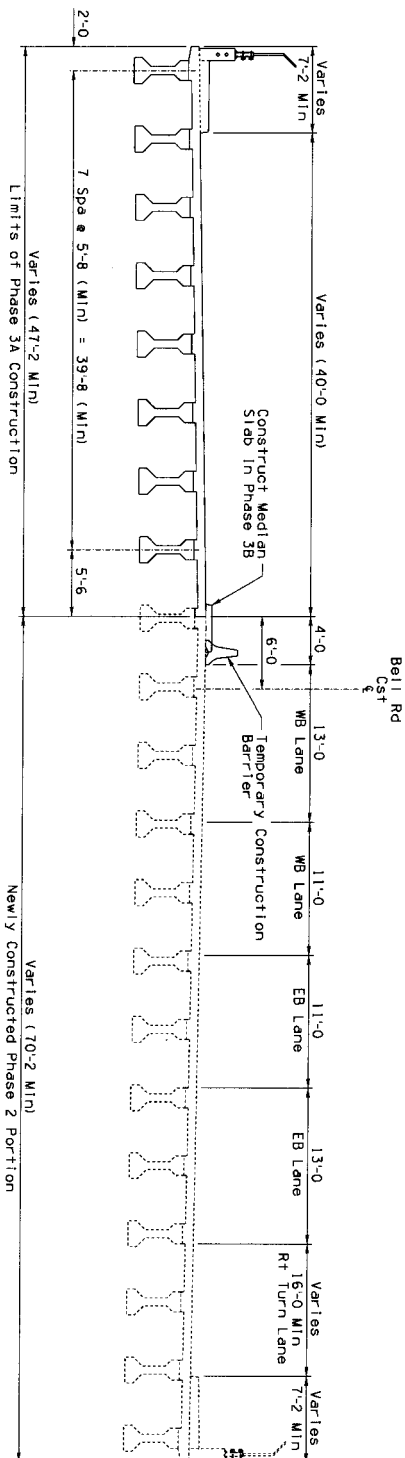
DESIGN	NAME	ARIZONA DEPARTMENT OF TRANSPORTATION	PHIL. IN-HAURY
NO. 1	S. THORNE	INTERNATIONAL TRANSPORTATION DIVISION	PLANS
DATE	1. 14. 1941	BRIDGE GROUP	FOR REVIEW
CREATED	16. 3. 1941	284-4	NOT FOR CONSTRUCTION
		BELL RD UP AT EB US 60	CONSTRUCTION
		TYPICAL SECTION & PROFILES	DRW. NO. 1-102
			DRW. NO. 2-102
US 60	TBD	US 60 (GRAND AVE) BELL RD TI	
ROUTE	143		
	STATIONING		
		060-B-NFA	
TRACKS NO. H4485 01C			OF

CONSTRUCTION PHASING NOTES:

- Phase 1A
See Traffic Plans for traffic shifts & pavement work.
- Phase 1B
Install temporary pavement (as required) on northern portion of Bell Road.
- Phase 2
Shift both directions of Bell Road traffic to the northern portion of Bell Road.
Construct the southern portion of the bridge.
- Phase 3A
Shift both directions of Bell Road traffic to the newly-constructed Phase 2 portion of the bridge.
Construct the northern portion of the bridge.
- Phase 3B
Temporarily shift WB traffic to the newly-constructed Phase 3A portion of the bridge & construct the median slab.



PHASE 2
Scale: 1" = 5'-0"

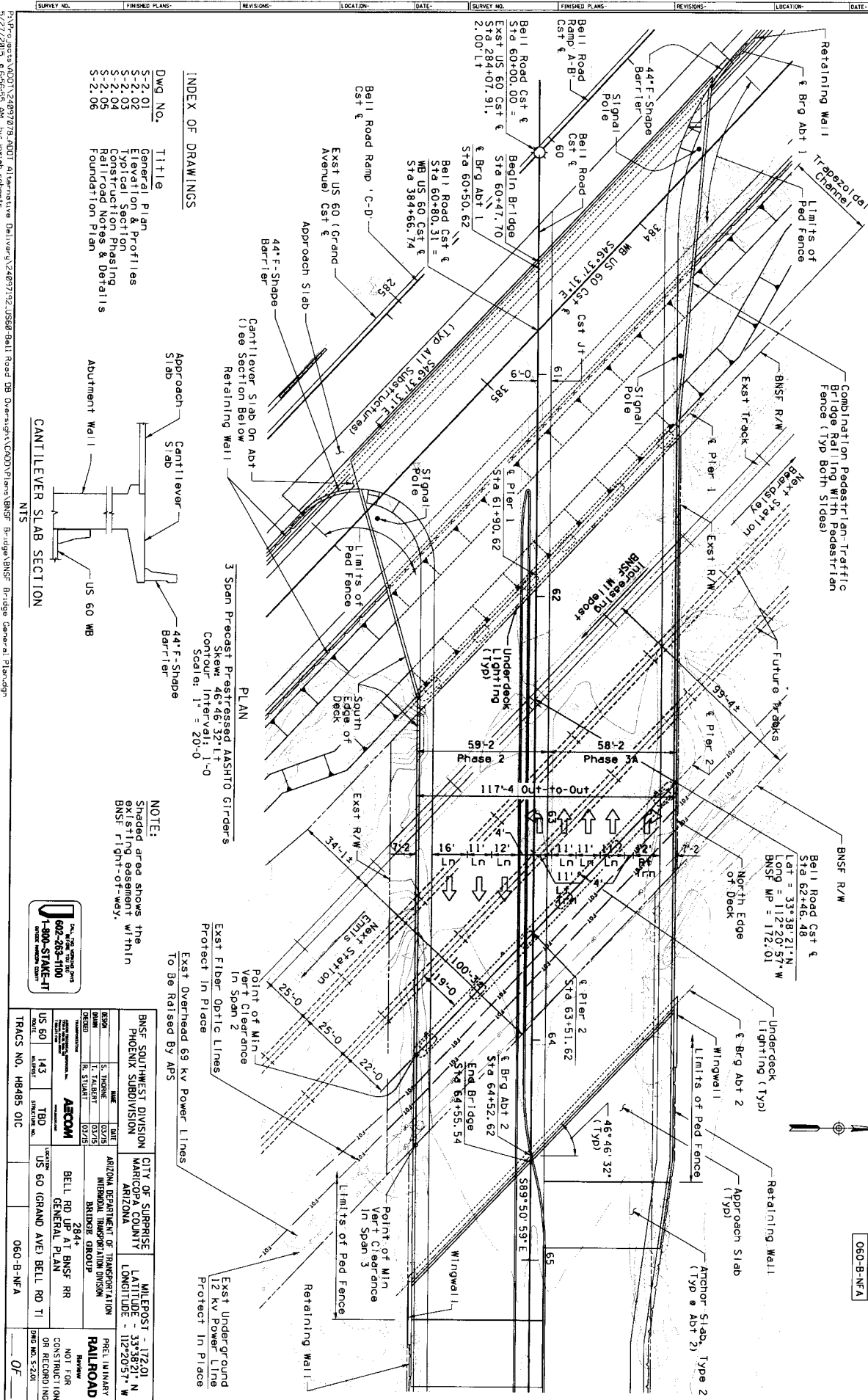


PHASE 3A
Scale: 1" = 5'-0"

REGION	STATE	PROJECT NO.	SHEET TOTAL	REV. DWS.
9	MA	060-B-NFA	NO. SHEETS	
60 MA 142				
NO.	DATE	BY	CHK.	DATE
US 60	143	TBD		
ANZONA DEPARTMENT OF TRANSPORTATION INTERMODAL TRANSPORTATION DIVISION BRIDGE GROUP				
2844 BELL RD UP AT EB US 60 CONSTRUCTION PHASING				
US 60 (GRAND AVE) BELL RD TI				
TRACS NO. H8485 OIC				
060-B-NFA				
PRELIMINARY PLANS NOT FOR CONSTRUCTION OR RECORDING DATE: 05/10/03				

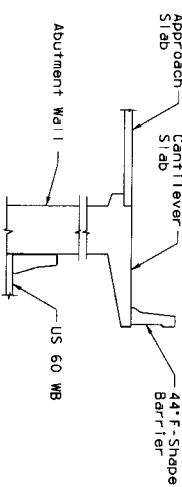
US 60 (GRAND AVENUE)
US60/BELL ROAD T1
MARICOPA COUNTY

TABLE	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	REV. DWS.
9	ARIZ.	60 MA 142			
060-B-NFA					



INDEX OF DRAWINGS

Dwg No.	Title
S-2.01	General Plan
S-2.02	Elevation & Profiles
S-2.03	Typical Section
S-2.04	Construction Phasing
S-2.05	Railroad Notes & Details
S-2.06	Foundation Plan

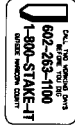


CANTILEVER SLAB SECTION

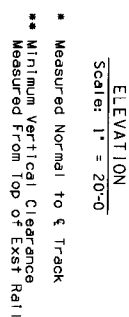
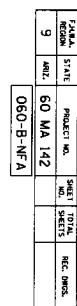
NTS

PLAN
3 Span Precast Prestressed AASHTO Girders
Skew: 46°46'32" LT
Contour Interval: 1'-0
Scale: 1" = 20'-0

NOTE:
Shaded area shows the
existing easement within
BNSF right-of-way.



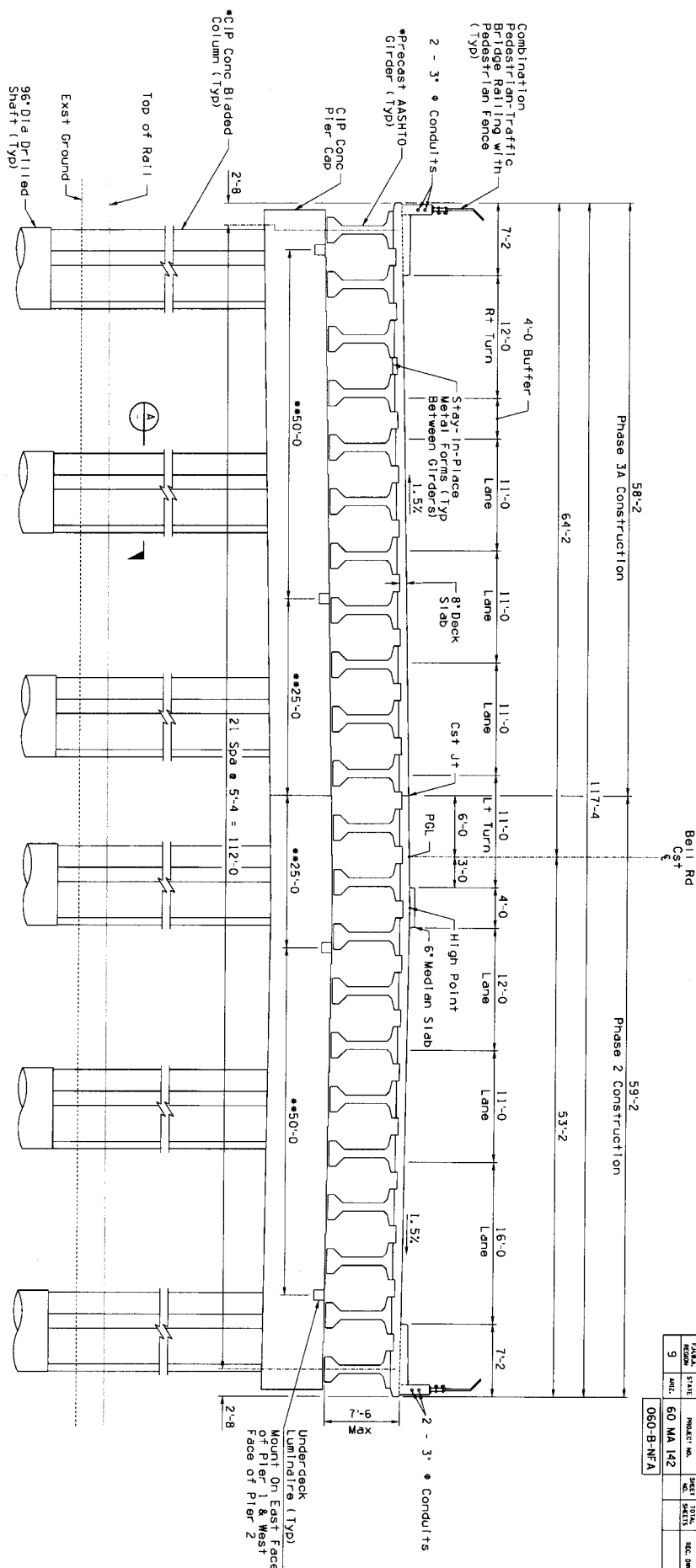
BNSF SOUTHWEST DIVISION PHOENIX SUBDIVISION		CITY OF SURPRISE MARICOPA COUNTY ARIZONA		MILEPOST - 112.01 LATITUDE - 33°38'21" N LONGITUDE - 112°20'57" W	
DESIGNED BY S. THORNE CHECKED BY J. STUBBS DATE 02/15 02/15		ARIZONA DEPARTMENT OF TRANSPORTATION BRIDGE GROUP		PRELIMINARY RAILROAD	
CONTRACT NO. US 60 143		TBD		NOT FOR CONSTRUCTION OR RECORDING DWS NO. S-2.01	
TRACKS NO. H8485 OIC		060-B-NFA		OF	



NOTE:

For Top of Rail Elevations, see Dwg No. S-2.05

BNSF SOUTHWEST DIVISION PHOENIX SUBDIVISION		CITY OF SURPRISE MARICOPA COUNTY ARIZONA		MILE POST - 172.01 LATITUDE - 33.8821° N LONGITUDE - 112°20.51' W	
ESCON	DATE	DATE	ARIZONA DEPARTMENT OF TRANSPORTATION	PRI. ILLINARY	RAILROAD Reactive NOT FOR CONSTRUCTION OR RECORDING
BLANK	5, THURSDAY	03/15	MIDLEVEL DIMENSIONALIZATION		
ORDERED	1, TUESDAY	03/15	BRIDGE GROUP		
	R, STURGE	03/15	384-		
TRANSMISSIONS	TRANSMISSION NO.		BELL RD UP AT BNSF RR ELEVATION & PROFILES		
US 60	143	TBD	LOCATION	US 60 (GRAND AVE) BELL RD TI	
DATE	APPROVED	STRUCTURAL NO.		060-B-NFA	
TRACKS NO. H8485 DIC					DATE NO. 5/2002
				OF	



*NOTES:

1. Precast AASH/O girders with cast-in-place concrete deck slab shown, vertical and horizontal clearances are permitted provided that final plans and specifications for construction activities or other obstructions are shown within the clearance envelope. In no case will cast-in-place superstructure types be permitted within the clearance envelope over the existing track.
2. Six columns with drilled shaft foundations are shown. Other layouts & number of columns & foundations are permitted provided that the final and temporary vertical & horizontal clearances are no less than as shown on the plans & that no construction activities or other obstructions are shown within the clearance envelope. If short spans required for the Railroad's publication "Guidelines for Temporary Shoring", dated October 25, 2004.
3. At Pier 2, columns shall be of heavy construction in accordance with section 5.5.2 of Railroad Guidelines or shall be protected by a pier protection wall.

TYPICAL SECTION - SPAN 2

Scale: 1" = 5'-0"

TYPICAL BLADED COLUMN SECTION

Scale: $\gamma_2 = 1-C$

BNSF SOUTHWEST DIVISION				CITY OF SUPRISE				MILEPOST - 33.3821			
PHOENIX SUBDIVISION				MARICOPA COUNTY				LATITUDE - 11°20'57" N			
				ARIZONA				LONGITUDE - 112°20'57" W			
				ARIZONA DEPARTMENT OF TRANSPORTATION				PHEIL IN CHARGE			
				METHODICAL INSPECTION DIVISION				RAILROAD			
				INDUSTRY GROUP				RAILROAD			
				BELL RD 1/4 BNSF RR				CONSTRUCTION			
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SURVEY NO.	FINISHED PLANS-	REVISIONS-	LOCATION-	DATE-	SURVEY NO.	FINISHED PLANS-	REVISIONS-	LOCATION-	DATE-
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Phase 2

Construct the southern portion of the bridge.

Phase 3A

Shift both directions of Bell Road traffic to the newly-constructed Phase 2 portion of the bridge.
Construct the northern portion of the bridge.

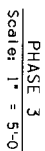
Phase 3B




Temporarily shift traffic on the bridge & construct the median slab.



PHASE 2

Scale: 1" = 5'-0"



BNSF SOUTHWEST DIVISION PHOENIX SUBDIVISION			CITY OF SURPRISE MARICOPA COUNTY ARIZONA			MILE POST - 33.8 LATITUDE - 33°21' N LONGITUDE - 112°20' W		
DATE	MADE	BY	ARIZONA DEPARTMENT OF TRANSPORTATION INTERMODAL TRANSPORTATION DIVISION			PERS. UNITS		
DESIGN	S. THORNE	10/2/78	BRIDGER GROUP			RAILROAD		
DESIGNED	R. STAFFITZ	10/2/78	284+ BNSF RR CONSTRUCTION PHASE			CONSTRUCTION NO. 28404		
								
US 60	143	TBD	US 60 (GRAND AVE) BELL RD 11			0660 B-NFA		
TRAFFIC NO. 18485 OIC						OF		

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5/27/2015 6:55:58 AM by: josh.roberts

1. All dimensions are measured perpendicular to the track.
 2. Prior to commencing any work, the contractor shall submit for approval by the railroad, detailed plans indicating the nature & extent of the track protection shoring proposed. The contractor shall install the temporary shoring system per the approved plans. Design of the temporary shoring system shall comply with Railroad guidelines for temporary shoring.
 3. For excavations which encroach into zone A or B, shoring plans shall be accompanied by design calculations. Plans & calculations must be signed & stamped by a professional engineer in the state of Arizona.
- RAILROAD NOTES:**
1. Railroad's review and approval of shoring, demolition, erection, and falsework are required. Allow a minimum of four weeks for the review and approval of each submittal.
 2. Any shoring system that impacts the Railroad's operation and/or supports the Railroad's embankment shall be designed and constructed per Railroad guidelines for Temporary Shoring.
 3. All demolition within the Railroad's right-of-way and/or demolition that may impact the Railroad's tracks or operation shall comply with the Railroad's Demolition requirements.
 4. Erection over the Railroad's track shall be planned such that it enables the track to remain open to traffic per Railroad requirements.
 5. The elevation of the existing top-of-rail profile shall be verified before beginning construction. All discrepancies shall be brought to the attention of the Railroad prior to construction.
 6. The proposed grade separation project shall not increase the quantity and/or characteristics of the flow the Railroad ditches and/or drainage structures. The Contractor must submit a proposed shoring plan and method of construction and have the method approved by the Railroad prior to beginning any grading on the project site.
 7. For Railroad coordination please refer to the Railroad's Coordination Requirements as part of the Specifications or Special Provisions of the project.
 8. Minimum Construction Clearance Envelopes of 21 feet vertical above the plane of top-of-rail and 15 feet horizontal at right angle from centerline of track shall be maintained at all times during construction. All permanent clearances shall be verified before project closeout.

RAILROAD NOTES:

1. Railroad's review and approval of shoring, demolition, erection, and falsework are required. Allow a minimum of four weeks for the review and approval of each submittal.

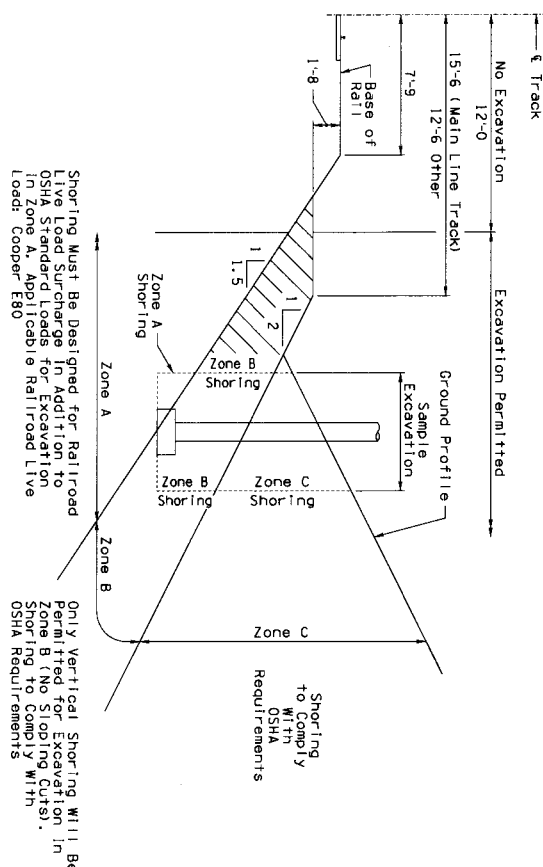
2. Any shoring system that impacts the Railroad's operation and/or supports the Railroad's embankment shall be designed and constructed per Railroad guidelines for Temporary Shoring.
3. All demolition within the Railroad's right-of-way and/or demolition that may impact the Railroad's tracks or operation shall comply with the Railroad's Demolition requirements.
4. Erection over the Railroad's track shall be planned such that it enables the track to remain open to traffic per Railroad requirements.

5. The elevation of the existing top-of-rail profile shall be verified before beginning construction. All discrepancies shall be brought to the attention of the Railroad prior to construction.

6. The proposed grade separation project shall not increase the quantity and/or characteristics of the flow in the Railroad ditches and/or drainage structures. The Contractor must submit a proposed method of erosion and sediment control and have the method approved by the Railroad prior to beginning any grading on the project site.

7. For Railroad coordination please refer to the Railroad's Coordination Requirements as part of the Specifications or Special Provisions of the project.

8. Minimum construction clearance envelopes of 121 feet vertical above the plane of top-of-rail and 15 feet horizontal at right angle from centerline of track shall be maintained at all times during construction. All permanent clearances shall be verified before project closeout.

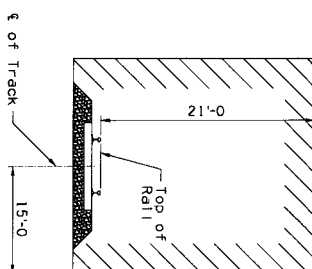


GENERAL EXCAVATION ZONES

25

Shoring Must Be Designed for Railroad Live Load Surcharge in Addition to OSHA Standard Loads for Excavation in Zone A. Applicable Railroad Live Load: Cooper E80

Only Vertical Shoring Will Be Permitted for Excavation in Zone B (No Sloping Cuts). Shoring to Comply with OSHA Requirements



MINIMUM CONSTRUCTION CLEARANCES

(Normal to Railroad
NTS)

BNSF SOUTHEAST DIVISION						CITY OF SURPRISE		MILEPOST - 17201
PHOENIX SUBDIVISION						MARICOPA COUNTY		LATITUDE - 33-38'29" N
						ARIZONA		LONGITUDE - 112-20'57" W
AZIMUTH DEPARTMENT OF TRANSPORTATION						RAILROAD		
INTERSTATE TRANSPORTATION DIVISION						PRELIMINARY		
BRIDGES GROUP						DRAWING NO.		
2044 T BNSF R						DATE		
BELL RD 1/4 SECTION 10						SHEET NO.		
RAILROAD NOTES & DETAILS						CONSTRUCTION		
ON RECORDED IN						DWS NO. 2,205		
US 60 (GRAND AVE) BELL RD TI						OF _____		
SECTION NO.						SCALE		
TRAFFIC NO. HB485 DIC						DATE		

Alignment: Left	Alignment: Right
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91+00 1207.47	91+00 1207.49
92+00 1206.94	92+00 1206.96
93+00 1206.48	93+00 1206.49
94+00 1206.02	94+00 1206.02
95+00 1205.40	95+00 1205.41
96+00 1204.78	96+00 1204.80
97+00 1204.09	97+00 1204.10
98+00 1203.39	98+00 1203.39
99+00 1202.50	100+00 1202.49
100+00 1201.93	101+00 1201.90
101+00 1201.49	102+00 1201.46
102+00 1201.03	103+00 1201.01
103+00 1200.58	104+00 1200.57
104+00 1200.25	105+00 1200.22
105+00 1199.85	106+00 1199.84
106+00 1199.39	107+00 1199.36
107+00 1198.96	108+00 1198.96
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110+00 1197.88	111+00 1197.88

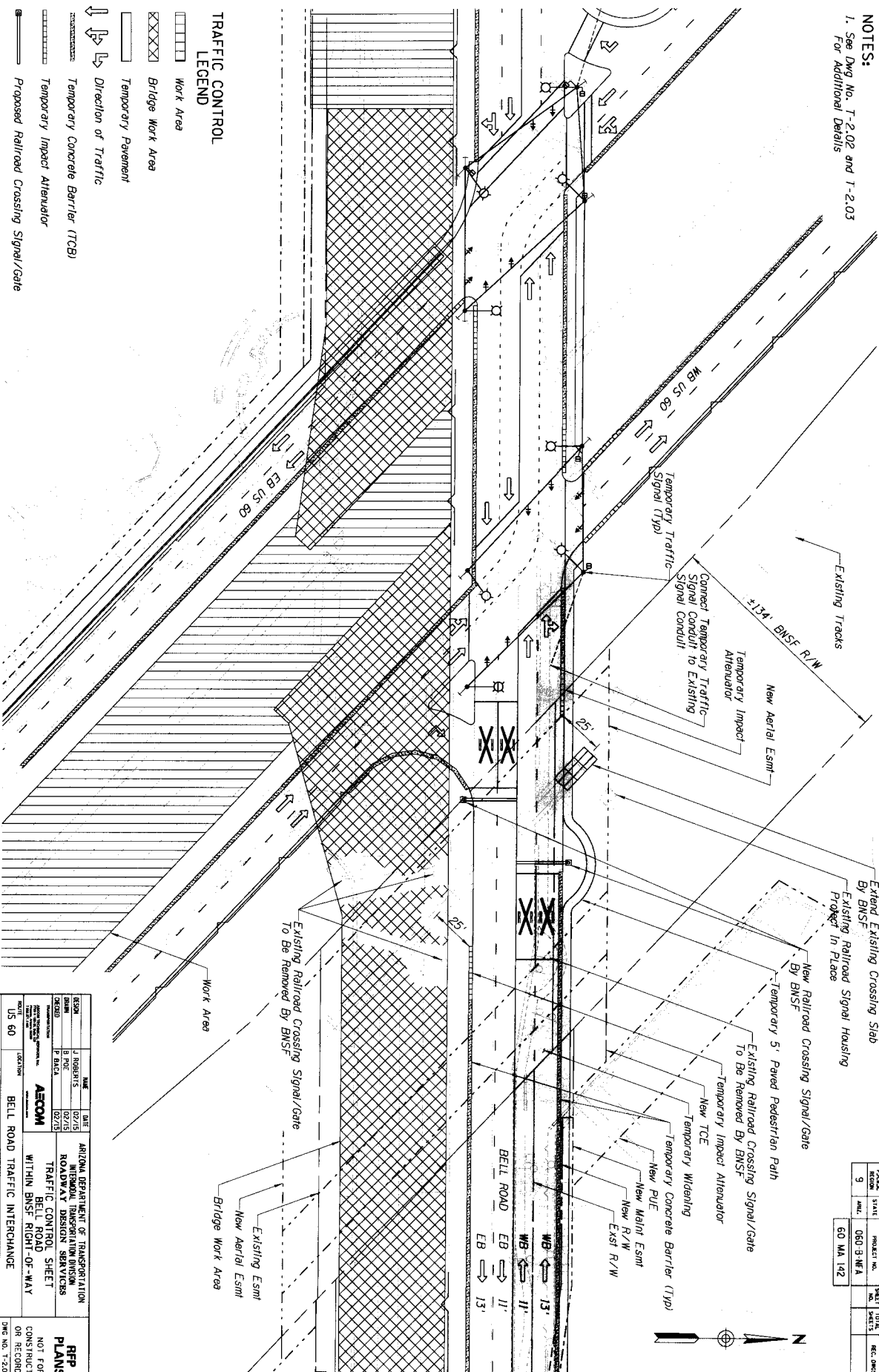
TOP OF RAIL ELEVATIONS

① Existing Track at Bell Road Cst &

No Construction Activities
Or Other Obstructions Shall
Be Placed Within These Limits

No Exceptions Allowed To
Contractor For Minimum Clearances

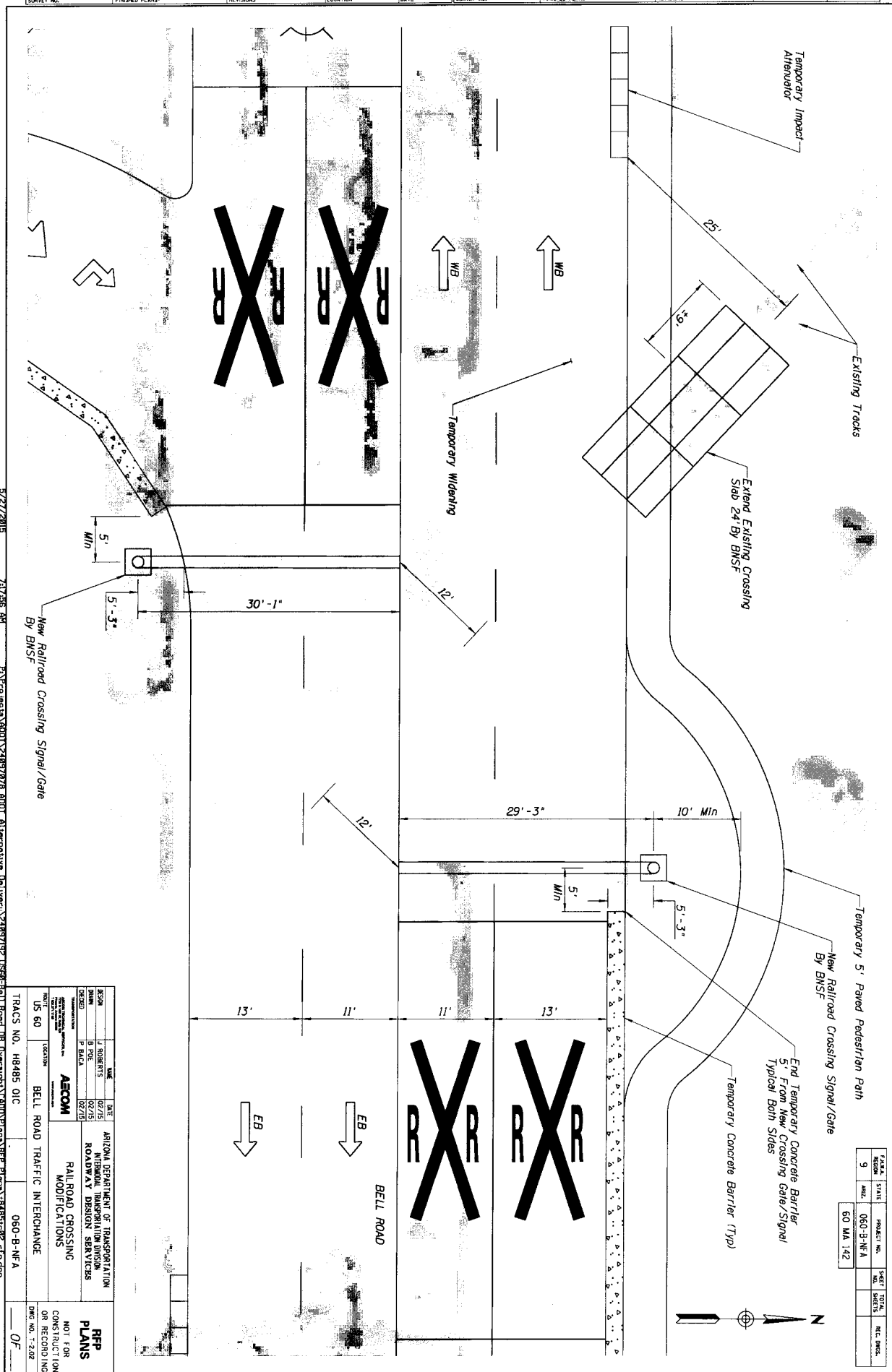
1. See Dwg No. T-2.02 and T-2.03
For Additional Details



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9	ARIZ.	060-B-NFA			
60 MA 142					

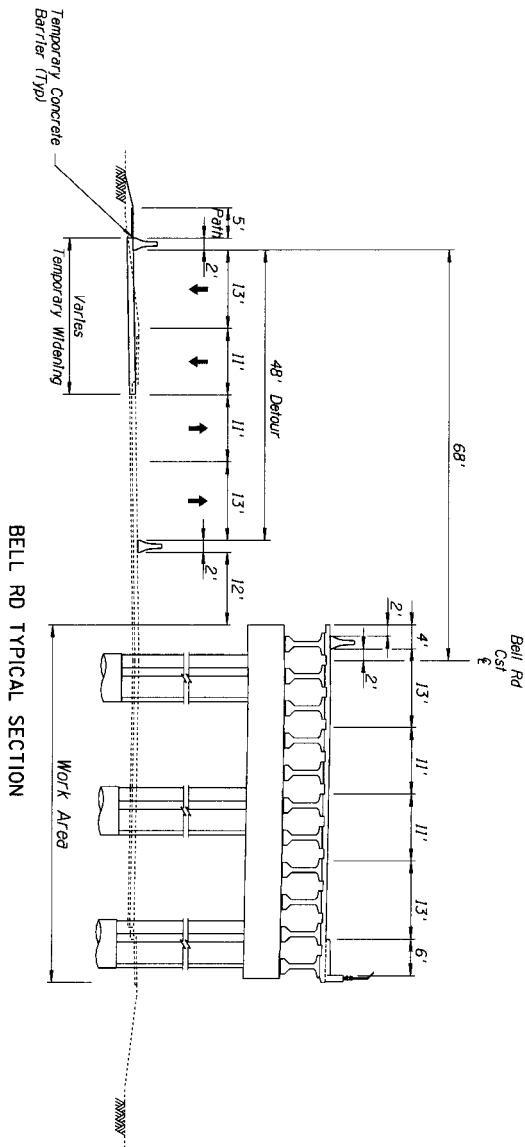
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F.Y.R.A. REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	REC. DWS.
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BELL RD TYPICAL SECTION

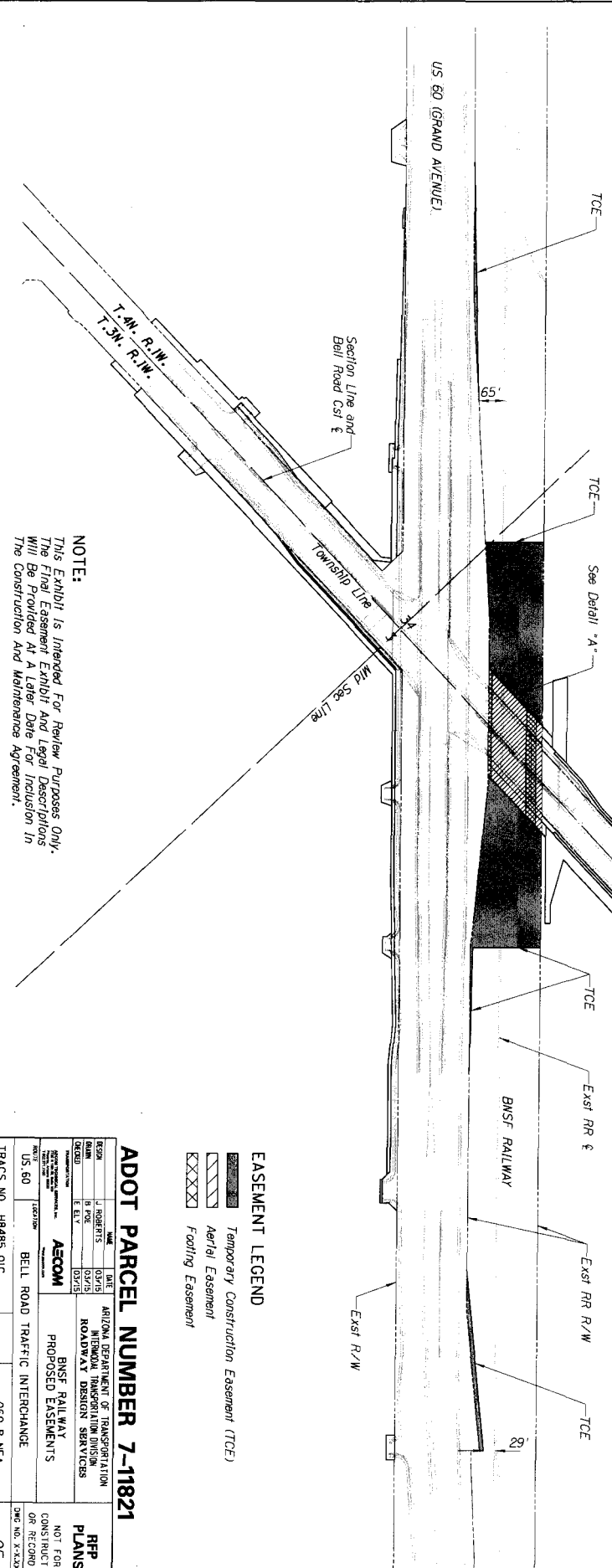
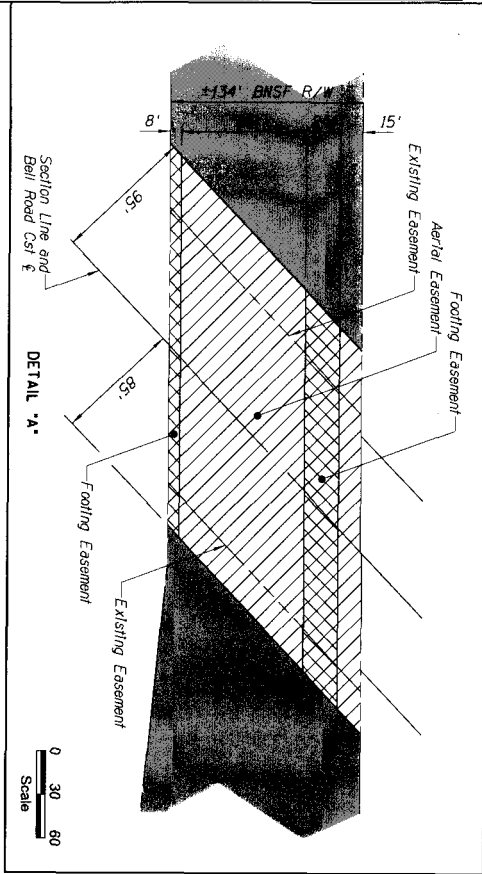
PLAN	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	REC. DATES
9	ARIZ.	060 B-NFA	142		
60 MA 142					

DESIGN	J. ROBERTS	DATE	02/15	ARIZONA DEPARTMENT OF TRANSPORTATION
DRAWN	B. POE	DATE	02/15	ROADWAY DESIGN SERVICES
CHECKED	P. BLACA	DATE	02/15	
AECOM 1000 N. GILBERT AVENUE, SUITE 100 MESA, AZ 85204-4099 TEL: 480-948-8800 FAX: 480-948-8801 WWW.AECOM.COM				RFP PLANS NOT FOR CONSTRUCTION OR RECORDING
PROJECT		TRAFFIC CONTROL SHEET		
LOCATION		TYPICAL SECTIONS		
US 60		WITHIN BNSF RIGHT-OF-WAY		
BELL ROAD TRAFFIC INTERCHANGE				
TRACS NO. H8485 OIC		060-B-NFA		
		OF		

5/27/2015

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NOTE:
This Exhibit is intended for Review Purposes Only.
The Final Easement Exhibit And Legal Descriptions
Will Be Provided At A Later Date For Inclusion In
The Construction And Maintenance Agreement.

5/27/2015 7:05:43 AM P:\Projects\ADOT\21097076 ADOT Alternative Delivery\21097192.1558 Bell Road DB Design\TCD001\Exhibits\BNSF Easement Exhibit\BNSF Easement Exhibit.dgn

- EASEMENT LEGEND**
- Temporary Construction Easement (TCE)
 - Aerial Easement
 - Footing Easement

ADOT PARCEL NUMBER 7-11821

ADOT PARCEL NUMBER 7-11821 TRACS NO. H8485 OIC		060-B-NFA OF	
REGION DISTRICT COUNTY ELEV. DATES	NAME DATE COMMENTS ROADWAY DESIGN SERVICES BNSF RAILWAY PROPOSED EASEMENTS	RFP PLANS NOT FOR CONSTRUCTION OR RECORDING DWG NO. X-XXX	
AECOM		ARIZONA DEPARTMENT OF TRANSPORTATION INTERMODAL TRANSPORTATION DIVISION ROADWAY DESIGN SERVICES	

PROJECT NO. 060-B-NFA 60 MA 142	SHEET TOTAL 12 OF 13 REC. DMS.
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